



CITY OF BISHOP

377 West Line Street - Bishop, California 93514
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Draft Negative Declaration of Environmental Impact

Date: December 28, 2012

Project Title: Warren Street Improvement Project

Project Proponent: City of Bishop
Department of Public Works
377 West Line Street
P.O. Box 1236
Bishop, CA 93515
760-873-5458

Project Contact Person: David Grah, Director of Public Works

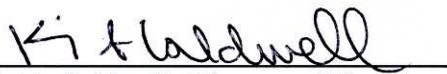
Project Location: In the City of Bishop, County of Inyo, State of California on South Warren Street and North Warren Street from South Street to one block north of Elm Street and on South, Lagoon, Church, Academy, Pine and Elm Streets between Main Street and Warren Street.

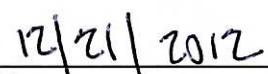
Project Description: The project will rehabilitate pavement, improve drainage, construct continuous curb, gutter and sidewalk, construct drive entrances and curb ramps, improve intersections, replace and extend underground storm drain facilities, repair and improve water and sewer facilities, relocate overhead utilities, remove and replace trees, construct bulb-outs and pocket parks, provide decorative street lighting, and provide an aesthetic theme to the street.

Proposed Findings: The Initial Study finds that the proposed project would not have a significant adverse impact on the environment. There would be no significant cumulative impacts, or substantial adverse impacts on human beings, or substantial adverse impacts on fish or wildlife or sensitive species or cultural resources. No significant adverse impacts are foreseen, and no mitigation measures are required.

The City of Bishop has determined that the project could not have a significant effect on the environment, and a Negative Declaration will be prepared. This Initial Study has been prepared to generally describe the proposed project and solicit input from agencies and the public regarding the scope of the proposed project.

The review period for this Draft Negative Declaration ends on January 28, 2013.


Keith Caldwell, Director of Planning


Date



DRAFT Initial Study
Warren Street Improvements Project
City of Bishop
Inyo County, California



Lead Agency:

City of Bishop
Department of Public Works
377 West Line Street
Bishop, CA 93514
760-873-8458

December 2012

SECTION 1 INTRODUCTION

1.1 PURPOSE

The purpose of the project is to address issues with deteriorated pavement, poor drainage, lack of accessibility for pedestrians, disabled persons, and bicycles, lack of sidewalks, broken, damaged and missing curb and gutter, aging water and sewer facilities, and lack of coherent and aesthetic theme. The street pavement is in poor condition and is reaching the end of its useful life. The deterioration of the pavement is exacerbated by poor surface drainage in some locations. Runoff from precipitation, irrigation, and other sources ponds on the roadway for extended periods. The ponded water freezes during the winter. There is little sidewalk and gutter. Many portions of the existing curb are in poor condition. What sidewalk exists is not continuous nor does it comply with accessibility standards. Drainage and pavement conditions present mobility problems for bicycles. Water and sewer systems are deteriorating and can leak and overflow. The project will also allow for the installation of an underground communication link between City of Bishop facilities at City Hall and the Public Works Shop on Warren Street. The specific transportation goals of the project are further described in the Project Study Report which was prepared for the project for Caltrans programming, included in the Attachments to this document.

1.2 PROJECT LOCATION

The project is located in the City of Bishop limits on North and South Warren Street from West South Street to a dead end one block north of West Elm Street, a distance of about 2,700 feet. West South Street, Lagoon Street, Church Street, Academy Street, West Pine Street, and West Elm Street between Warren Street and Main Street are also included in the project.

The project is in the Northwest $\frac{1}{4}$ of Section 7 and the Southwest $\frac{1}{4}$ of Section 6 Township 7 South Range 33 East Mount Diablo Baseline and Meridian.

See attached Warren Improvements Project map for location of project.

1.3 PROJECT DESCRIPTION

The scope of work for the project is to remove and replace pavement, remove and replace curb, gutter and sidewalk, reconstruct & extend the storm drain between Lagoon Street and Academy Street, construct cross gutters, construct curb ramps, provide street trees, irrigation and

architectural street lighting, improve street intersection and street grades to address drainage problems, construct bulb-outs at intersections to provide enhanced pedestrian refuge, traffic calming and areas for large shade trees. Some existing trees will be removed in order to complete the improvements. Some relocation of overhead utilities is expected. There may be some work outside the right of way in order to match grades behind sidewalks and at driveways. There will also be some water and sewer line improvements. The project intends to relocate overhead utilities including electrical power, phone and cable and intends to improve their aesthetics. Due to lack of funding to complete all proposed improvements at once, the project construction may be phased.

The proposed project is shown on the attached Concept Layout.

1.4 PROJECT PROPONENT

City of Bishop

Department of Public Works
377 West Line Street
Bishop, California 93514
760-873-8458

Contact: David Grah, Director of Public Works

1.5 INTENDED USES OF THIS DOCUMENT

The City of Bishop will use this Initial Study to identify potential environmental impacts from the project and to solicit input regarding the project from agencies and the general public. This Environmental Initial Study will also be used in support of a Negative Declaration when considering the approval of the project.

1.6 ENVIRONMENTAL SETTING

The City of Bishop is located in Inyo County at the northern end of Owens Valley. The City covers an area of approximately 1.8 square miles and has a population of approximately 3,879 (United States Census 2010). The population is expected to remain relatively steady because it is largely prevented from growth because the City is surrounded by a combination of public and Native American lands. The City of Bishop was incorporated in 1903.

The Owens River, which is located east of the City of Bishop, flows to the south down the valley. The valley is bounded by the Sierra Nevada mountain range to the west and the White

Mountains range to the east. Numerous creeks and ditches carry water from the Sierra Nevada Mountains toward the Owens River.

Bishop is located in the rain shadow of the Sierra Nevada. The warmest month of the year is July with an average maximum temperature of about 98 degrees Fahrenheit. The coldest month of the year is December with an average minimum temperature of 22 degrees Fahrenheit. Temperature variations between night and day are over 40 degrees during the summer and over 30 degrees during winter. The annual average precipitation at Bishop is 5 inches. The wettest month of the year is February with an average rainfall of 1 inch.

The project is at an elevation of approximately 4,140 feet and slopes slightly to the east. This slight slope to the east does not promote drainage on Warren Street which is oriented north-south.

Warren Street is classified as a Commercial Street and is generally characterized by commercial businesses and governmental services, although there are a few residences fronting the street. West South Street and West Elm Street are classified as Urban Collector streets. Lagoon Street, Church Street, Academy Street, and Pine Street are classified as Local Commercial Streets.

1.7 GENERAL PLAN DESIGNATION

Most of the proposed work is within city street right of way that is not zoned. Warren Street is surrounded by property zoned C-1 (Commercial – General/Retail). Warren Street is identified as a Commercial Street in the Bishop General Plan. The previously mentioned side streets which are a part of the project are all adjacent to C-1 zoning, except South Street which has a portion of C-2 (General Commercial) adjacent to it.

In addition, the blocks between Warren Street and Main Street and between Lagoon Street and Academy Street are part of the Downtown Core zoning overlay.

See attached City of Bishop Zoning map, Circulation Plan, and Circulation Standards for zoning and general plan information.

SECTION 2 ENVIRONMENTAL CHECKLIST

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
1. Aesthetics				
<i>Would the project:</i>				
a) Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic building within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Agriculture Resources				
<i>In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland.</i>				
<i>Would the project:</i>				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Air Quality				
<i>Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.</i>				
<i>Would the project:</i>				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions, which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Biological Resources <i>Would the project:</i>				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
5. Cultural Resources				
<i>Would the project:</i>				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Geology and Soils				
<i>Would the project:</i>				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
7. Hazards and Hazardous Materials <i>Would the project:</i>				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the likely release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located within one-quarter mile of a facility that might reasonably be anticipated to emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Be located on a site of a current or former hazardous waste disposal site or solid waste disposal site unless wastes have been removed from the former disposal site; or 2) that could release a hazardous substance as identified by the State Department of Health Services in a current list adopted pursuant to Section 25356 for removal or remedial action pursuant to Chapter 6.8 of Division 20 of the Health and Safety Code?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be located on land that is, or can be made, sufficiently free of hazardous materials so as to be suitable for development and use as a school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
j) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Hydrology and Water Quality <i>Would the project:</i>				
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted?)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner, which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures, which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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9. Land Use and Planning				
<i>Would the project:</i>				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural communities conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. Mineral Resources				
<i>Would the project:</i>				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. Noise				
<i>Would the project result in:</i>				
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. Population and Housing				
<i>Would the project:</i>				
a) Induce substantial population growth in an area, either directly (e.g., by proposing new homes and businesses) or indirectly (e.g., through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. Public Services				
<i>Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</i>				
a) Fire Protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Police Protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. Recreation				
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. Transportation/Traffic				
<i>Would the project:</i>				
a) Cause an increase in traffic, which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Issues	Potentially Significant Impact	Less Than Significant With Mitigation	Less Than Significant Impact	No Impact
b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
16. Utilities and Service Systems <i>Would the project:</i>				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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17. Mandatory Findings of Significance				
a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Factors That Could Result in a Potentially Significant Impact		
The environmental factors listed below are not checked because the proposed project would not result in a "potentially significant impact" as indicated by the preceding checklist and supported by substantial evidence provided in this document.		
<input type="checkbox"/> Aesthetics	<input type="checkbox"/> Agriculture Resources	<input type="checkbox"/> Air Quality
<input type="checkbox"/> Biological Resources	<input type="checkbox"/> Cultural Resources	<input type="checkbox"/> Geology/Soils
<input type="checkbox"/> Hazards & Hazardous Materials	<input type="checkbox"/> Hydrology/Water Quality	<input type="checkbox"/> Land Use/Planning
<input type="checkbox"/> Mineral Resources	<input type="checkbox"/> Noise	<input type="checkbox"/> Population/Housing
<input type="checkbox"/> Public Services	<input type="checkbox"/> Recreation	<input type="checkbox"/> Transportation/Traffic
<input type="checkbox"/> Utilities/Services Systems	<input type="checkbox"/> Mandatory Findings of Significance	

Environmental Determination

On the basis of this initial evaluation:

- I find that the proposed project could not have a significant effect on the environment, and a **Negative Declaration** will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A **Mitigated Negative Declaration** will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an **Environmental Impact Report** is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measure based on the earlier analysis as described on attached sheets. An **Environmental Impact Report** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier **EIR** or **Negative Declaration** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier **EIR** or **Negative Declaration**, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signed Keith Caldwell
 Keith Caldwell
 City Administrator

Date 12/21/2012

SECTION 3

DISCUSSION OF ENVIRONMENTAL EVALUATION

1. AESTHETICS

The project site is characterized as a mixed use commercial street with a wide variety in the age and type of buildings. The building occupants which currently have frontage along Warren Street and the associated side streets include clothing and grocery stores, laundries, a library, a police station, a bed and breakfast, motels, County services, auto body and mechanic shops, storage units and a variety of other small businesses. There is also some residential use including two single family homes, a trailer park and some townhomes. The City of Bishop owns and operates five public parking lots along Warren Street. The streetscape is not uniformly landscaped, and the lack of continuous sidewalks reduces its appeal as a commercial center and pedestrian thoroughfare. There are some mature trees. The overhead utilities dominate the air space above the street level and occupy the west side of the street for about half of the length of Warren Street and the east side for the other half. Where not obscured by buildings, overhead utilities or trees, there are dramatic views of the surrounding mountains.

Trees planted over the years in the street right of way provide shade and aesthetic appeal in some areas. Unfortunately many of the same trees cause extensive damage to street and sewer improvements, occupy the only space available for sidewalk in the public right of way, and impede drainage. Some of the existing trees are non-acceptable species per the City of Bishop list of acceptable street trees. Trees were selected for this list based on their beneficial qualities and their limited impacts on improvements. The list of acceptable trees is contained in the attached City of Bishop Tree Care Information and Tree List. The construction of the proposed improvements will cause the removal of about 20 trees between 6 and 15 inches diameter at breast height. The trees to be removed are predominantly elm and ash. The project proposes to plant over 120 trees. Approximately one third of the trees would be a larger tree of about 3 inches in diameter at breast height. The varieties of the replacement trees would be a selection of trees from the Bishop list of acceptable trees.

Decorative street lights are proposed. The lights would be spaced regularly along the sidewalks and would be for the benefit of lighting the pedestrian pathway. The street lights will be dark sky friendly. The type and style of luminaire is yet to be decided, but will be a factor in determining the overall character of the street.

Certain types of street furniture including benches, trash cans and drinking fountains are also possible functional and aesthetic improvements which would accompany the street improvements.

Sidewalk construction will conform to City of Bishop standards for commercial streets which requires a 10-foot wide concrete sidewalk. However, in order to accommodate street trees, grated tree wells will be constructed within the 4 feet of sidewalk closest to the curb. In some areas, where practical, "bulb outs" will be constructed that increase the planter space to up to 13 feet. These bulb out locations could provide adequate space for planting larger replacement trees.

Almost all project construction will be within the city right of way. The project will not directly impact adjacent properties, nor directly impact the aesthetics of adjacent properties.

The impacts of this project to aesthetics will be less than significant.

2. AGRICULTURAL RESOURCES

The project site is developed city street right of way. The site does not contain Prime Farmland and is not under a Williamson Act Contract to be preserved as farmland.

The proposed project will have no negative impact on agricultural resources.

3. AIR QUALITY

Air Quality within the City of Bishop and surrounding Inyo County is monitored and regulated by the Great Basin Unified Air Pollution Control District. Inyo County is listed as attainment (within allowable limits) for the following criteria pollutants: ozone; carbon monoxide; nitrogen dioxide; sulfur dioxide; sulfates; hydrogen sulfide; and vinyl chloride. Inyo County is listed as non-attainment for the state standard for PM-10 (particulate matter less than 10 microns in diameter) air emissions, which include chemical emissions and other inhalable particulate matter with an aerodynamic diameter of less than 10 microns.

The project is not expected to increase traffic-related emissions. Air quality impacts would be limited to the emissions from construction equipment involved in the construction of the proposed improvements. These impacts would last the approximately 3 to 6 months of construction. The short duration of the proposed work combined with existing regulations

regarding motor vehicle fuels and emissions would result in potential air quality impacts being well below any state or federal significance criteria.

The project does not propose any use or construction technique that would result in odors that would be objectionable to the general public.

PM-10 emissions during construction would be controlled through the implementation of best management practices to limit PM-10 emission such as regular use of a water truck to keep potential dust producing surfaces damp.

In the short term, the removal of large trees will tend to decrease the amount of carbon dioxide absorbed and the amount of oxygen released by trees in the project area. This will be offset by a potential decrease in motor vehicle use due to the improvement of pedestrian and bicycle facilities in the area. As the replacement trees grow, the net decrease in carbon dioxide absorption and oxygen associated with the removal of large trees will diminish.

The proposed project will have no negative impact on air quality.

4. BIOLOGICAL RESOURCES

No critical habitat or special status species, sensitive species or species of concern have been identified within the proposed project area. Almost the entire project area is disturbed from its natural condition. The project area is populated with exotic and horticultural species of plants.

The trees in the project area are not native species and are not thought to be important habitat for raptors. No important or protected avian species are known to nest or forage on the project site. Tree removal shall be timed to occur between the beginning of October and the end of February to minimize impacts to nesting birds and roosting bats.

The City of Bishop General Plan Area does not include habitat, natural community, or other conservation plans. No conflicts are expected to occur.

The proposed project will have a less than significant impact on biological resources.

5. CULTURAL RESOURCES

The project site is developed public street right of way with no significant cultural features. There are no known historic or prehistoric cultural resources on the project site. If cultural resources are discovered during construction, construction activity will be immediately stopped and a qualified cultural specialist will be contacted.

The proposed project will have no negative impact on cultural resources.

6. GEOLOGY AND SOILS

Natural Resources Conservation Service (NRCS) soil survey for soils within the project area indicate the soils consist of Dehy loam 0 to 2 percent slopes. These soils are not considered to be expansive and are suitable for the subgrade of roadways and for the construction of pipelines.

The Bishop Area is located in seismic Zone 4. The project area is not an Alquist-Priolo Special Studies Zone. No special measures are required to address potential seismic activity in the area during construction or during use of the constructed product.

The proposed project will have no negative impact on geology and soils.

7. HAZARDS AND HAZARDOUS MATERIALS

The construction of the project and use of the constructed features will not pose any significant hazard to the public or the environment. Construction of the project will involve the short-term use of hazardous materials such as diesel fuel and grease associated with the construction equipment but the hazards of these materials are not substantially different from the hazards presented by similar materials now on the existing streets and in the future when the proposed improvements are complete. Refueling and equipment maintenance would be done off-site or within a contained area so as to avoid soil contamination on the project site. No long-term use of hazardous materials is foreseeable as a result of the project.

There are several automotive repair shops adjacent to the project. There is the potential for contaminated soil to exist in the vicinity of these shops due to leaking cars waiting to be repaired being parked on the street. There is also the potential for contaminated soil from historical gas stations which were located near or adjacent to the project boundaries. If soil contaminated with oil, gasoline, grease, radiator fluid or other hazardous materials associated with vehicles is

encountered during excavation and construction of the street improvements, the soil shall be removed and disposed of in a manner consistent with County landfill requirements and clean structural backfill shall be put in its place.

The proposed project will have no negative impact on hazards and hazardous materials.

8. HYDROLOGY AND WATER QUALITY

The project site is nearly level, almost entirely paved and the potential for erosion is low. Ponding is the greatest water-related issue on the project. The proposed project will improve drainage on Warren Street by construction of continuous curb and gutter, new cross gutters and reconstruction and extension of underground storm drain system.

The project includes the installation of a new stormwater separator which will collect and contain solid waste, sediments, and oils and will thereby increase the quality of the storm water runoff from the site.

During construction, the construction contractor would employ Best Management Practices (BMPs) for the containment of construction related materials.

The proposed project will have no negative impact on hydrology and water quality.

9. LAND USE AND PLANNING

Most of the project site is owned by the City of Bishop. Most of the site owned by the City of Bishop is street right of way intended for streets, sidewalk, drainage, water, sewer, and other utilities.

Most of the proposed work is within city street right of way that is not zoned. Warren Street and the associated side streets are mostly surrounded by property zoned C-1 (Commercial – General/Retail) with a very small portion being zoned C-2. Warren Street is identified as a Commercial Street in the Bishop General Plan. All proposed improvements are consistent with existing and proposed land use in the area.

The proposed project will have no negative impact on land use and planning.

10. MINERAL RESOURCES

The project will use mineral aggregate in base, paving, and concrete and reclaimed materials will be used if possible thereby minimizing demand on mineral resources. No mineral resources are known to exist on the project site.

The proposed project will have no negative impact on mineral resources.

11. NOISE

There is the potential for the project to promote increased use of the street including the use of the streets and pocket park areas for special events. This use could increase noise but would have to be in accordance with noise limits in the Municipal Code.

The proposed project would result in temporary noise associated with construction activities. Construction would be limited to 7:00 a.m. – 7:00 p.m.

The proposed project will have a less than significant impact on noise.

12. POPULATION AND HOUSING

There is no housing located on the project site and none is proposed. Most of the project site is public property (street right of way) intended for public uses other than housing.

The proposed project would not require or encourage an increase in population or the construction of housing. It is anticipated the project will provide significant assets to the neighborhoods, the city and the community.

The proposed project will have no negative impact on population and housing.

13. PUBLIC SERVICES

The proposed project would contribute to the City of Bishop's domestic water public service demands and, except for disposal of materials removed during construction such as existing pavement, would not require any other public services.

There is the potential for the proposed bulb-outs at some street intersections to restrict the turning movement of long wheel-based vehicles, specifically fire-fighting ladder trucks.

The proposed project will have no negative impact on public services.

14. RECREATION

Most of the project is city street right of way and so is not used for formal recreation. The project proposes to include small areas for pedestrian and public use. These areas are often referred to as a pocket park and also known as a mini-park or “parkette”. In addition, the extra area provided by bulb-outs at some intersections in conjunction with nearby pocket parks may increase the pedestrian use and recreational opportunities on the streetscape. Both pocket parks and bulb-outs provide areas large enough for large shade trees and benches which allow comfortable rest areas for pedestrians, shoppers and tourists.

The proposed project will have no negative impact on recreation.

15. TRANSPORTATION AND TRAFFIC

The proposed project is intended to enhance the city's transportation system including pedestrian and bicycle travel. Construction of the project will increase traffic and related impacts during construction but will be of short duration.

The project has the potential for short-term impact to access of businesses on Warren Street, the associated side streets and Main Street. A large portion of public parking lots which serve all these streets are accessed on Warren Street. Scheduling and phasing construction activities will be important to provide continuous access to parking as well as ingress and egress of places of business as much as possible during the construction of the project.

In some locations there are extensive areas of almost continuous driveways that can cause conflicts with continuous sidewalks. To the extent practical, defined and limited driveways will be created.

"Bulb outs" would reduce the amount of on-street parking available, however there is a large volume of parking spaces in any of five separate public parking lots on Warren Street. In addition, the side streets connecting to Warren also have on-street parking available. Bulb-outs

may have the potential to restrict the turning movement of long wheel-based vehicles, such as delivery trucks and fire trucks.

The proposed project will have a less than significant impact on transportation and traffic.

16. UTILITIES AND SERVICE SYSTEMS

The proposed project is intended to enhance water, sewer, and drainage facilities in the area. The relocation of some overhead and the potential relocation of some underground private utilities such as power, phone, and cable is anticipated. The project proposes to provide for new underground communication lines between the City of Bishop's Department of Public Works facilities.

The proposed project will have no negative impact on utilities and service systems.

17. MANDATORY FINDINGS OF SIGNIFICANCE

Project impacts would mostly be short-term and minor. The proposed project would not cause any potential impacts to the environment that could result in a mandatory finding of significance.

SECTION 4 ATTACHMENTS

Site Photographs

Project Location Map

Conceptual Layout

City of Bishop Commercial Street Section

City of Bishop Zoning map

City of Bishop Circulation Plan

City of Bishop Standards for Landscaping Within the Public Rights of Way

City of Bishop Tree Care Information and Tree List

Project Study Report (PSR)



Existing trees in front of City of Bishop Parking Lot 1



Looking north on Warren Street from South Street



No curb ramp. Utility pole conflict. No gutters.
Northwest corner of Warren Street and Academy Street



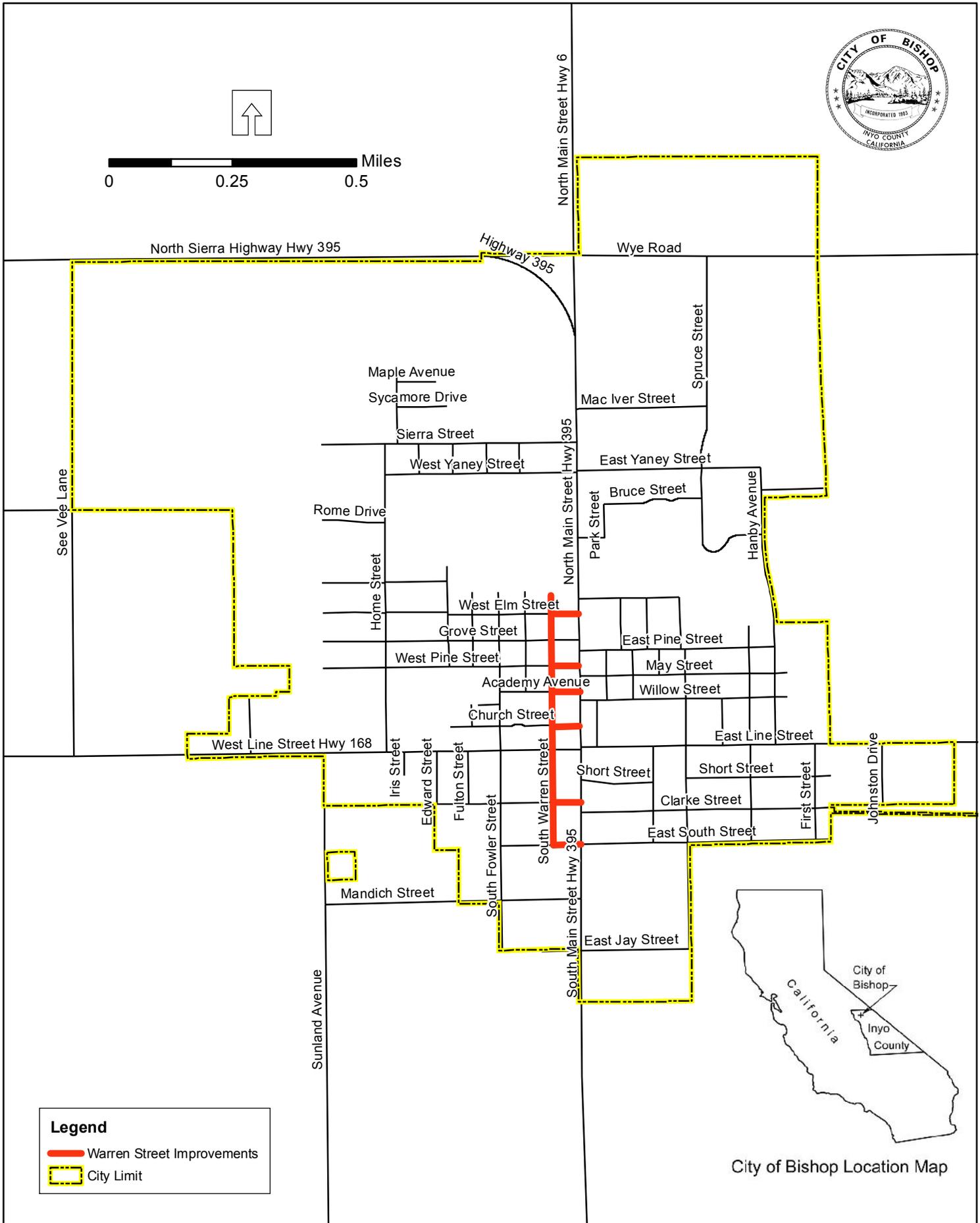
Lack of continuous sidewalk. No gutter.
Facing south. East side of Warren Street north of Lagoon Street.



Lack of curb gutter and sidewalk
Facing south. East side of Warren Street, north of Elm Street



Failing street asphalt. Lack of curb gutter and sidewalk.
Facing south. East side of Warren Street, north of Grove Street



City of Bishop Location Map

Legend

- Warren Street Improvements
- City Limit

Warren Street Improvements Project

City of Bishop



NOT TO SCALE

CITY OF BISHOP
DEPARTMENT OF PUBLIC WORKS
CORPORATE YARD

FUTURE
PUBLIC
RESTROOMS

CITY PARKING LOT 13
(60 spaces in front & 84 spaces in back)

TRAILER PARK

236 LAGOON ST.

N STREET

S0°14'37"E 1011.83'

TORAGE UNITS

LAGOON STREET

JIM ALLEN'S
AUTOMATICS

GENESIS MARKET

JCPENNY

CITY PARKING LOT
12
(30 spaces)

S

COBWEBS ANTIQUES

GREAT BASIN
BAKERY

ALPINE SIGNS

ONEY'S
BAR-B-Q
BILLS

LA CASITA

BISHOP
CLOTHING CO.

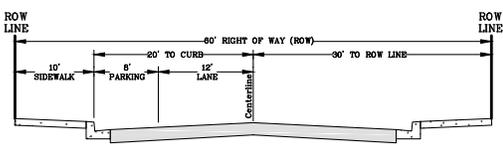
INYO COUNCIL
FOR THE ARTS

BEADS
BISHOP
JEWELERS

BANNER

MAIN STREET

NOT TO SCALE



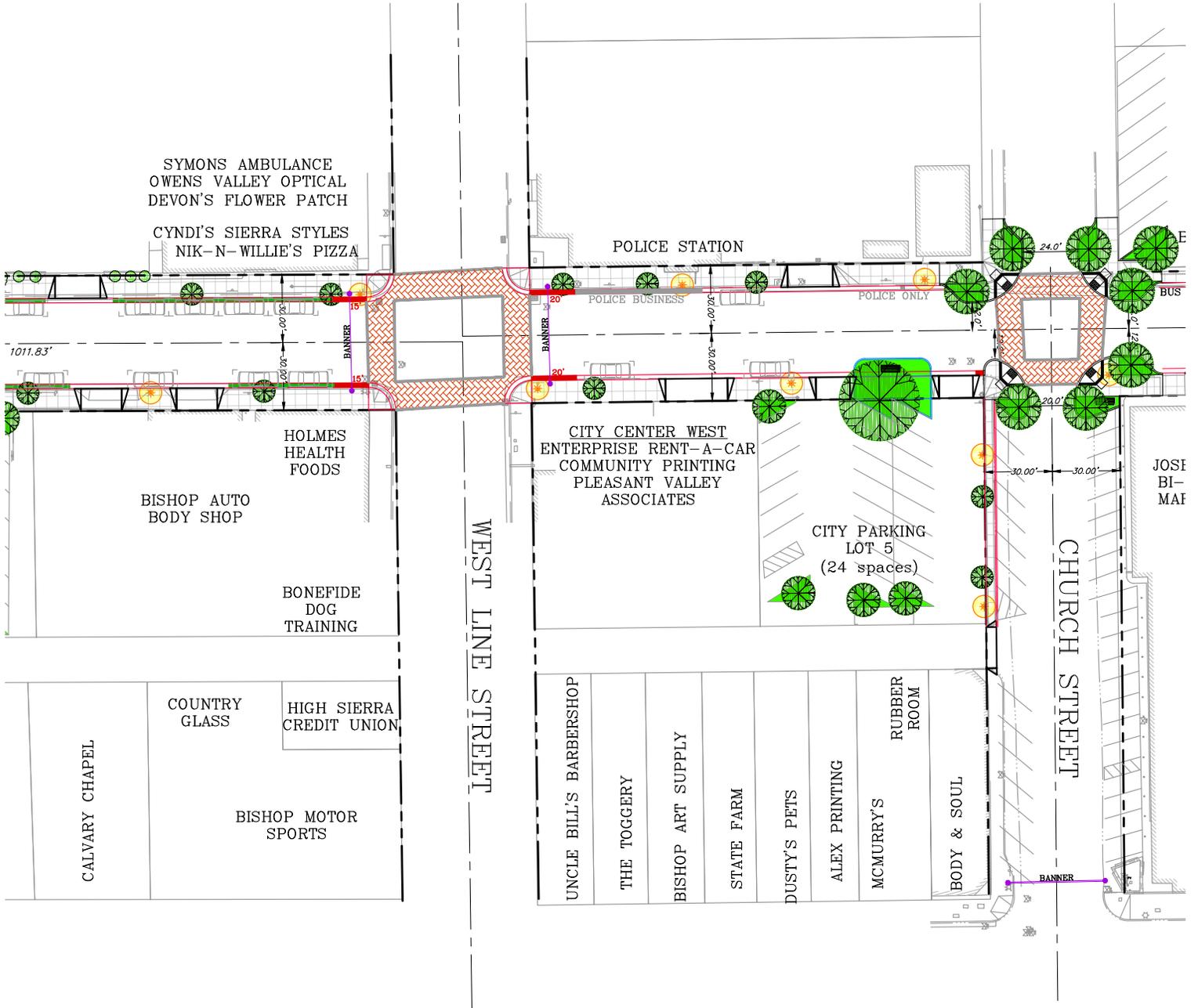
Notes:
Pavement is 4 inches asphalt concrete over 8 inches aggregate base
Curb and gutter is Caltrans A2-6 over 6 inches aggregate base
Sidewalk is 4 inches concrete over 4 inches aggregate base
Sidewalk and curb are poured monolithically
Driveways follow Caltrans standard plan A87A
Residential driveways are over 4 inches aggregate base
Commercial driveways are over 8 inches aggregate base
Pavement and sidewalk cross slope is 2%

CITY OF BISHOP
COMMERCIAL STREET SECTION

WARREN STREET

STREET

CONCE PLAN.



LEGEND

-  PROPOSED CURB AND GUTTER
-  PROPOSED RED CURB
-  PROPOSED CONCRETE DRIVEWAY
-  PROPOSED LANDSCAPED AREA
-  PROPOSED DECORATIVE CROSSWALK
-  PROPOSED FENCE/SCREENING
-  PROPOSED TREE
-  EXISTING TREE TO REMAIN
-  PROPOSED ORNAMENTAL STREET LIGHT
-  PROPOSED BENCH
-  PROPOSED BANNER
-  PROPOSED 8'x22' PARALLEL PARKING

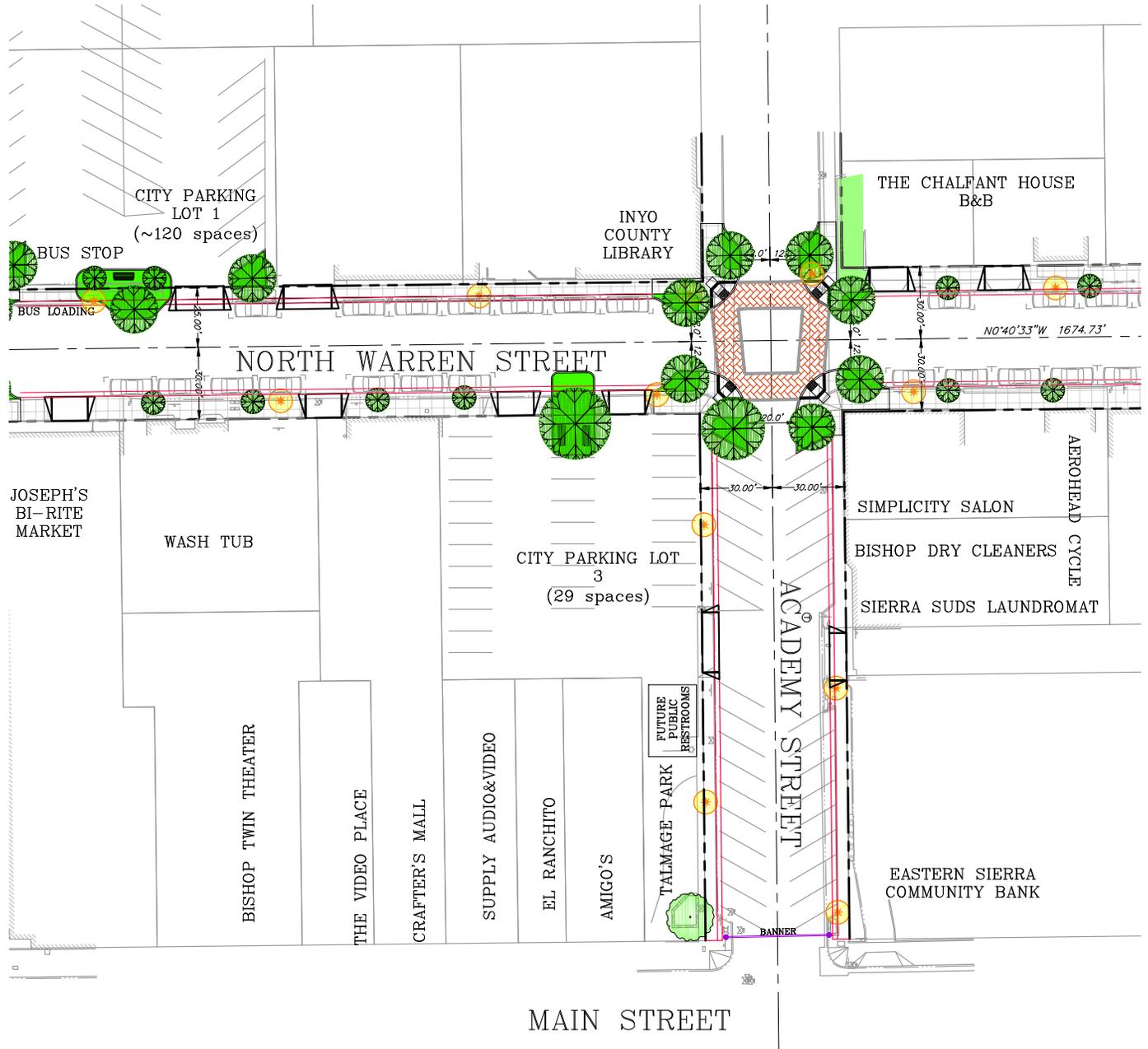
NOT TO SCALE

CONCEPTUAL

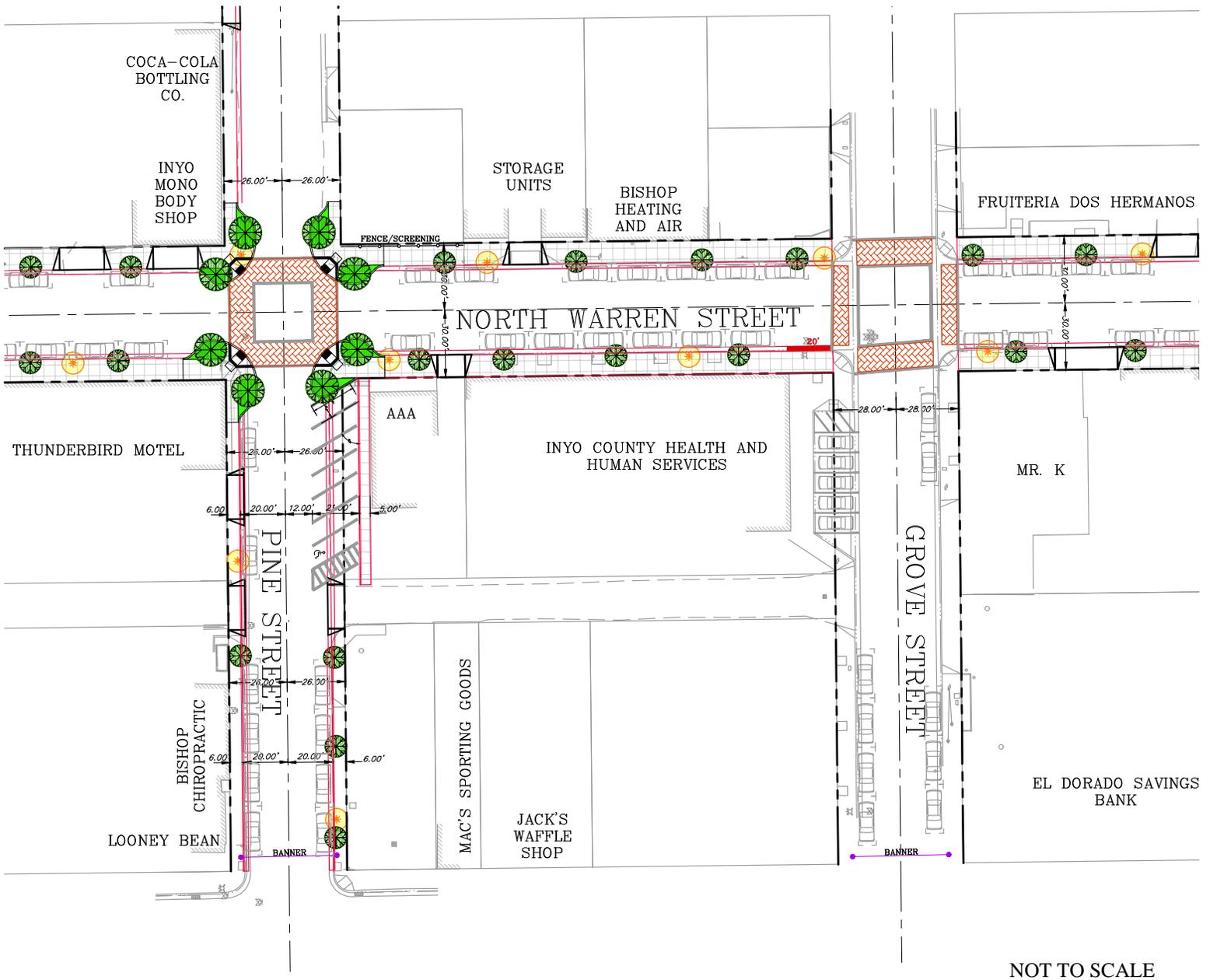
IMPROVEMENTS

SEPT 1: TWO WAY TRAFFIC

N DATE: 24 AUGUST 2012

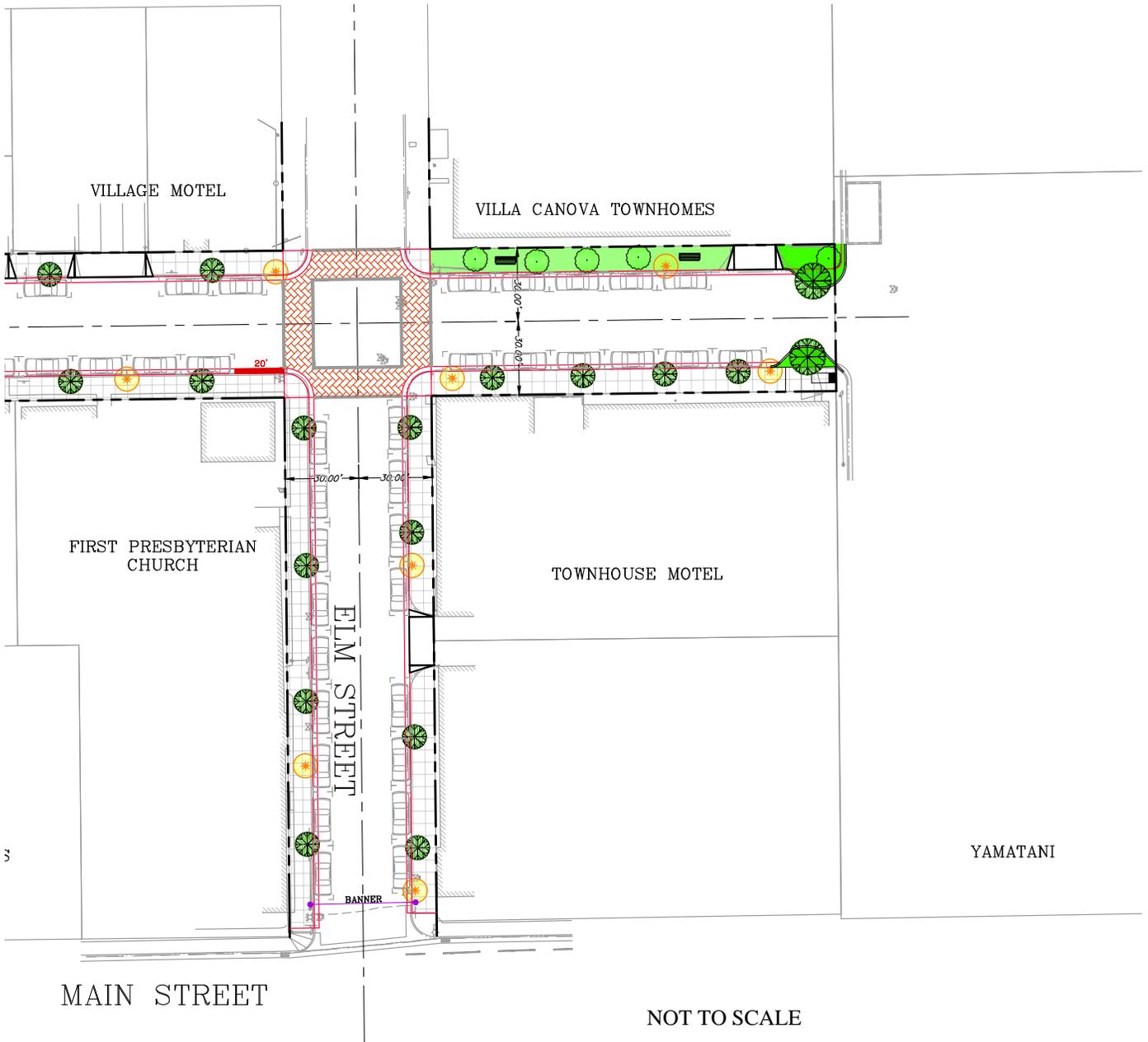


NOT TO SCALE



PARKING SUMMARY
(WARREN STREET, CITY PARKING LOTS AND SIDE STREETS FROM MAIN ST. TO WARREN ST.)

EXISTING PARKING		PROPOSED PARKING		NET GAIN/LOSS	
WARREN STREET	122 SPACES	WARREN STREET	118 SPACES	WARREN STREET	-4 (GAINED 6, LOST 10)
SOUTH STREET	14 SPACES	SOUTH STREET	11 SPACES	SOUTH STREET	-3
LAGOON STREET	15 SPACES	LAGOON STREET	13 SPACES	LAGOON STREET	-2
CHURCH STREET	34 SPACES	CHURCH STREET	34 SPACES	CHURCH STREET	0
ACADEMY STREET	30 SPACES	ACADEMY STREET	30 SPACES	ACADEMY STREET	0
PINE STREET	17 SPACES	PINE STREET	17 SPACES	PINE STREET	0
GROVE STREET	14 SPACES	GROVE STREET	14 SPACES	GROVE STREET	0
ELM STREET	17 SPACES	ELM STREET	17 SPACES	ELM STREET	0
CITY LOT 1	120 SPACES	CITY LOT 1	120 SPACES	CITY LOT 1	0
CITY LOT 3	29 SPACES	CITY LOT 3	27 SPACES	CITY LOT 3	-2
CITY LOT 5	24 SPACES	CITY LOT 5	24 SPACES	CITY LOT 5	0
CITY LOT 12	30 SPACES	CITY LOT 12	28 SPACES	CITY LOT 12	-2
CITY LOT 13	124 SPACES	CITY LOT 13	124 SPACES	CITY LOT 13	0
TOTAL =	590 SPACES	TOTAL =	577 SPACES	TOTAL =	-13 SPACES



NOT TO SCALE

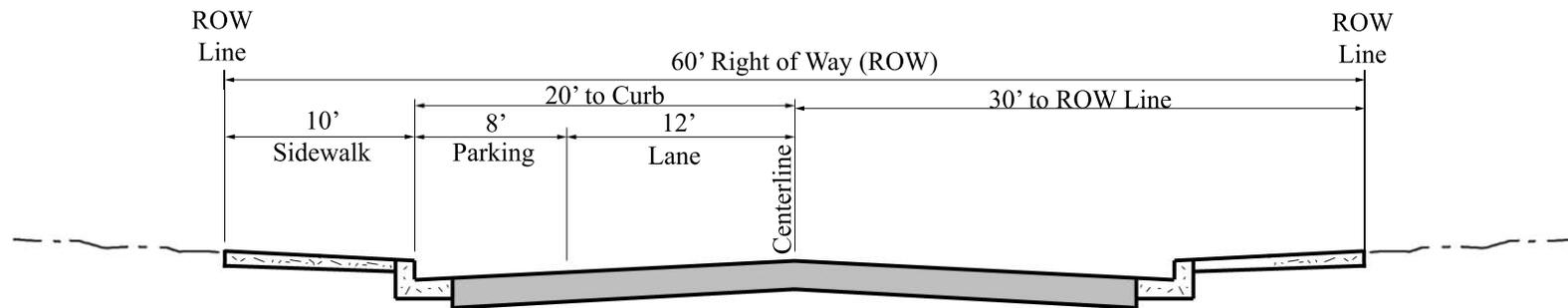


CITY OF BISHOP
 377 West Line Street - Bishop, California 93514
 Post Office Box 1236 - Bishop, California 93515
 760-873-8458 publicworks@ca-bishop.us
 www.ca-bishop.us

triad/holmes
 associates
 civil engineering
 surveying
 public works
 land development
 873 NORTH MAIN STREET #150
 BISHOP, CA 93514
 phone (760) 873-4273
 fax (760) 873-8024
 email triad@thainc.com

**WARREN STREET IMPROVEMENTS
 CONCEPTUAL PLAN**

24 AUGUST 2012
 Warren St Concept.dwg



City of Bishop Commercial Street Section

Notes:

- *Pavement is 4 inches asphalt concrete over 8 inches aggregate base
- *Curb and gutter is Caltrans A2-6 over 6 inches aggregate base
- *Sidewalk is 4 inches concrete over 4 inches aggregate base
- *Sidewalk and curb are poured monolithically
- *Driveways follow Caltrans standard plan A87A
- *Residential driveways are over 4 inches aggregate base
- *Commercial driveways are over 8 inches aggregate base
- *Pavement and sidewalk cross slope is 2%

City of Bishop General Plan

MOBILITY ELEMENT

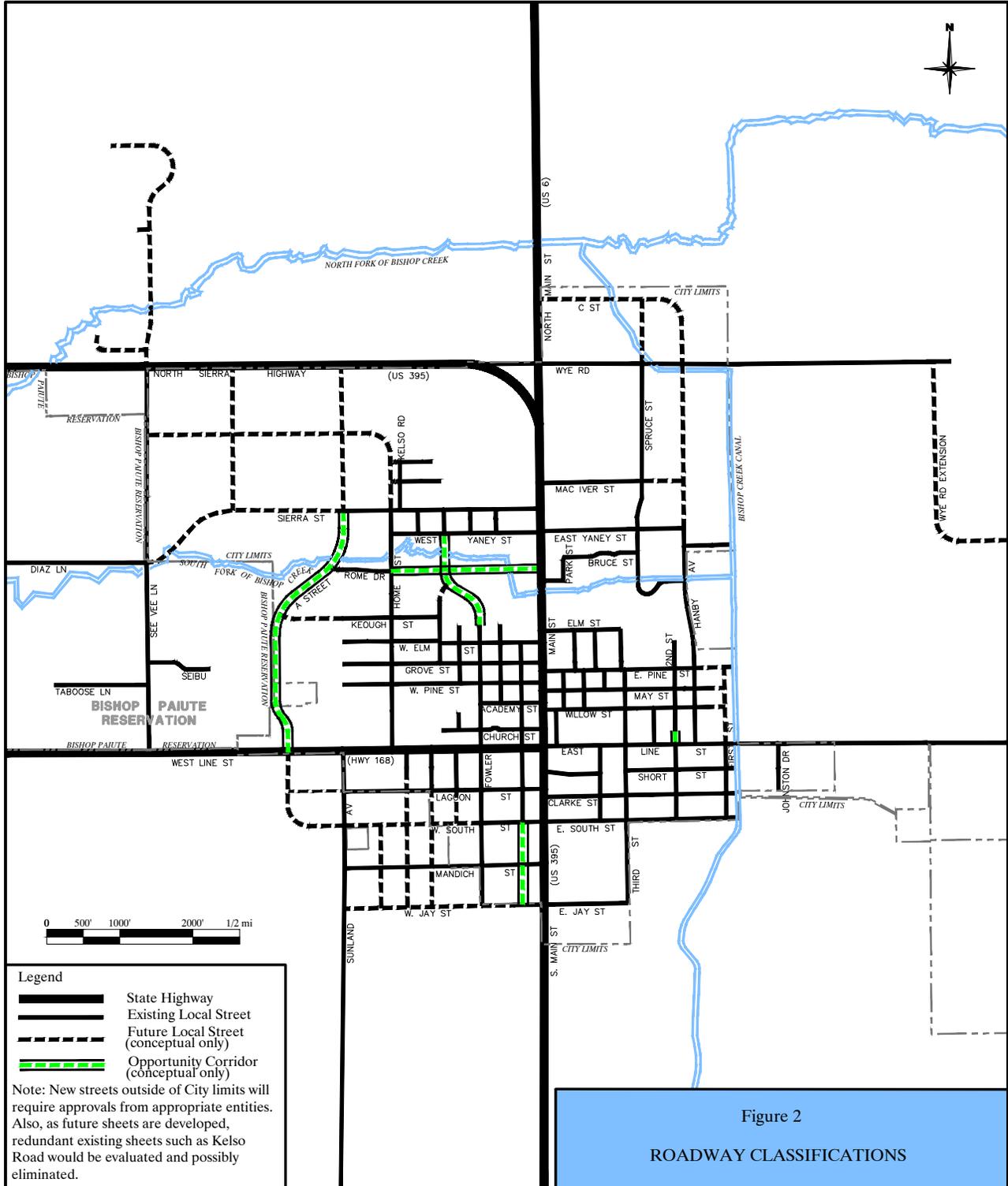


Figure 2
ROADWAY CLASSIFICATIONS

CITY OF BISHOP

STANDARDS FOR LANDSCAPING WITHIN THE PUBLIC RIGHTS-OF-WAY

1. The City of Bishop encourages landscaping public streets. These standards are intended to promote the organized development of healthy trees and landscaping within the public rights-of-way with minimal damage to curb, gutter, sidewalk, structures and utilities.

It is noted that in accordance with the Streets and Highway Code, property owners are responsible for all improvements between the property line and the curb line.

2. Root barrier shall be placed in locations shown on the standard drawings to a minimum depth of 18" below the surface. Root barrier shall be installed linear style or surround style as approved by the City. Mechanical root barrier shall be polystyrene plastic, or similar material, with added ultraviolet inhibitors, and a minimum wall thickness of 0.060". Chemical root barriers shall contain time-release chemicals that control root growth for a minimum of 12 years and shall be constructed to a minimum depth of 18" below the surface.. Chemical barriers shall be EPA registered and cleared for use in California; shall have an EPA toxicity classification of IV with an Oral LD 50 level greater than 5,000; and shall be on the list of barriers approved by the City of Bishop Department of Public Works. All barriers shall be installed in accordance with the manufacturers requirements and these standards.

3. Planter size shall be a minimum 4 ft square; or in a linear planter shall be a minimum 4 ft wide strip.

4. Planters shall be equipped with facilities for deep watering to establish deep root systems. Irrigation shall conform to City standards for water-efficient landscapes (Chapter 13.07 Bishop Municipal Code).

5. Tree well covers shall be used in areas zoned commercial unless waived in writing by the City. Covers shall be manufactured high density plastic or cast iron. Decorative brick may be used if approved by the City. In residential areas, tree well areas may be covered with dirt, gravel, bark, brick or commercial covers. Linear planting strips may use grass, potentilla or other landscaping approved by the City.



Approved By
City Administrator
Andrew Boyd
MAY 29, 1996
Date

TREE STANDARDS

Approved By
City Engineer
Andrew Boyd
FEB 22, 1996
Date

No.	Date	App.	Revision

Standard Detail No.
L-1
1 OF 5

6. Trees shall not be located within 15 ft of fire hydrants; 10 ft of curb returns, driveways or utility poles; or 5 ft of underground utility services.

7. All trees shall be staked until they are established and can stand alone. Stakes shall be located not to interfere with pedestrian or street traffic.

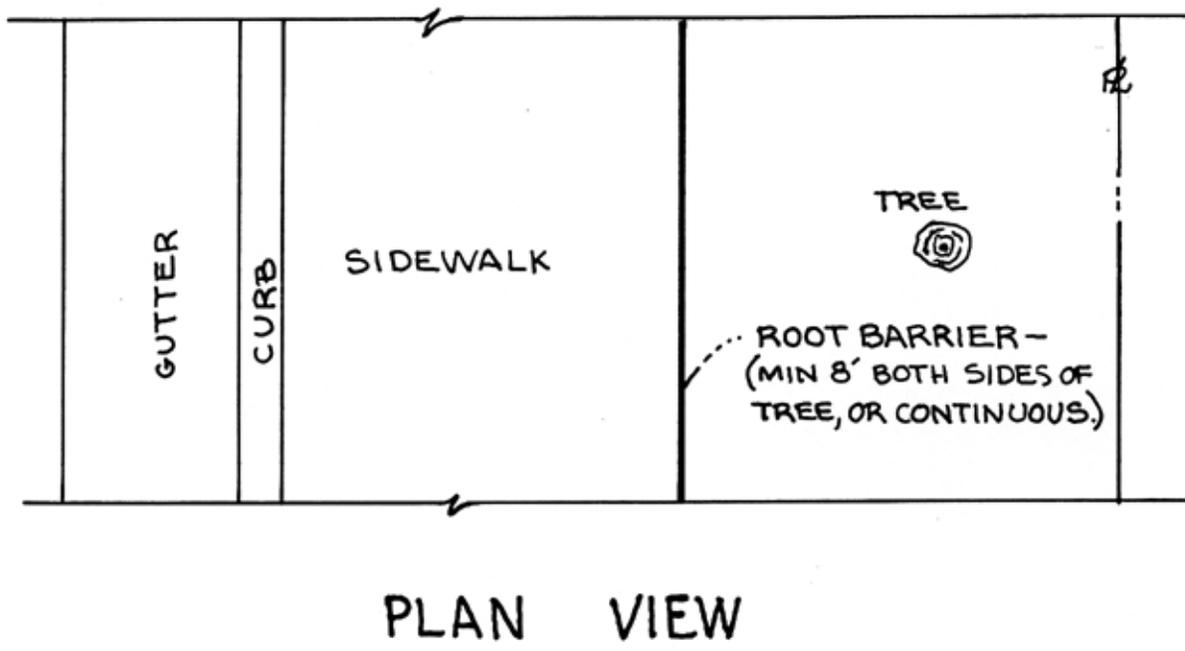
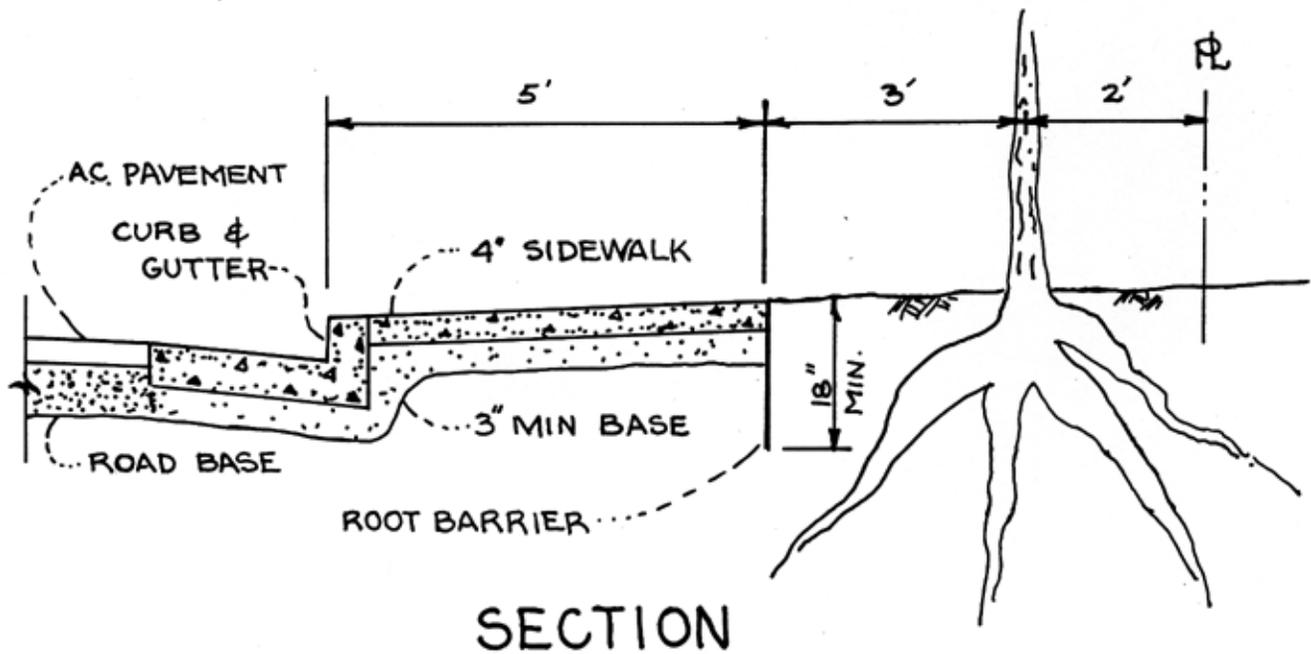
8. Trees that are acceptable for planting include:

- PYRUS calleryana "Bradford" (Flowering Pear)*
- PRUNUS serrulata kwanzan (Flowering Cherry)*
- PRUNUS cerasifera "Thundercloud" (Flowering Plum)
- PURNUS persica (Flowering Peach)
- ACER rubrum (Red Maple)*
- ARBUTUS unedo (Strawberry Tree)
- CERCIS canadensis (Eastern Redbud)
- ROBINIA ambigua "Idahoensis" (Idaho Locust)
- PISTACIA chinensis (Chinese Pistache)

* Not recommended for planting under power lines or telephone lines.

The above trees are suitable for street landscaping and will have minimal impact on public improvements and utilities if they are cared for properly. No other trees will be planted within the right-of-way unless approved in writing by the City.

	Approved By City Administrator <i>[Signature]</i> MAY 29, 1996 <small>Date</small>	TREE STANDARDS				
	Approved By City Engineer <i>[Signature]</i> FEB 22, 1996 <small>Date</small>					Standard Detail No. L - 2 2 OF 5
		<small>No.</small>	<small>Date</small>	<small>App.</small>	<small>Revision</small>	



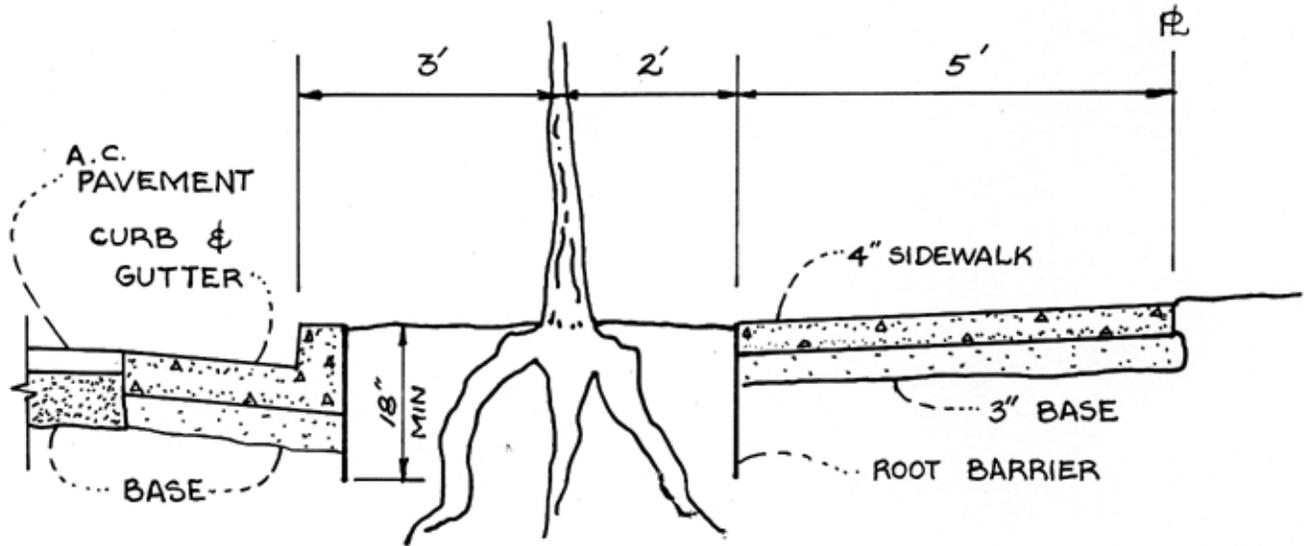
Approved By
City Administrator
Philad...
Date
MAY 29, 1996

Approved By
City Engineer
Andrew Boyd
Date
FEB 22, 1996

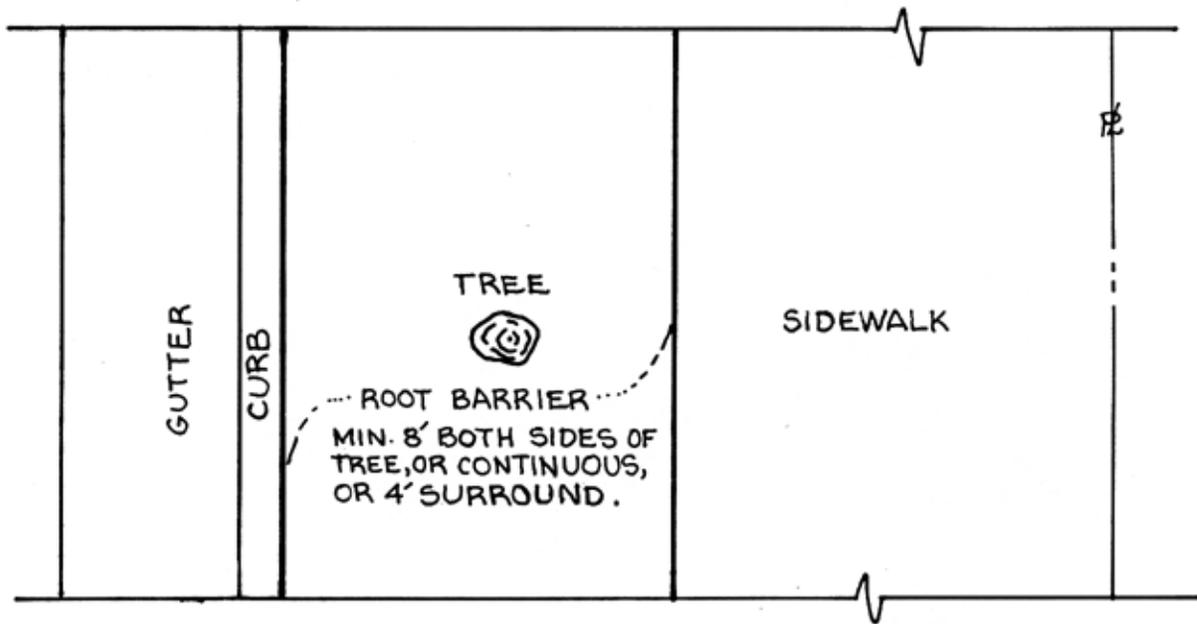
RESIDENTIAL TREE - TYPE 'A'

No.	Date	App.	Revision

Standard Detail No.
L-3
3 OF 5



SECTION



PLAN VIEW



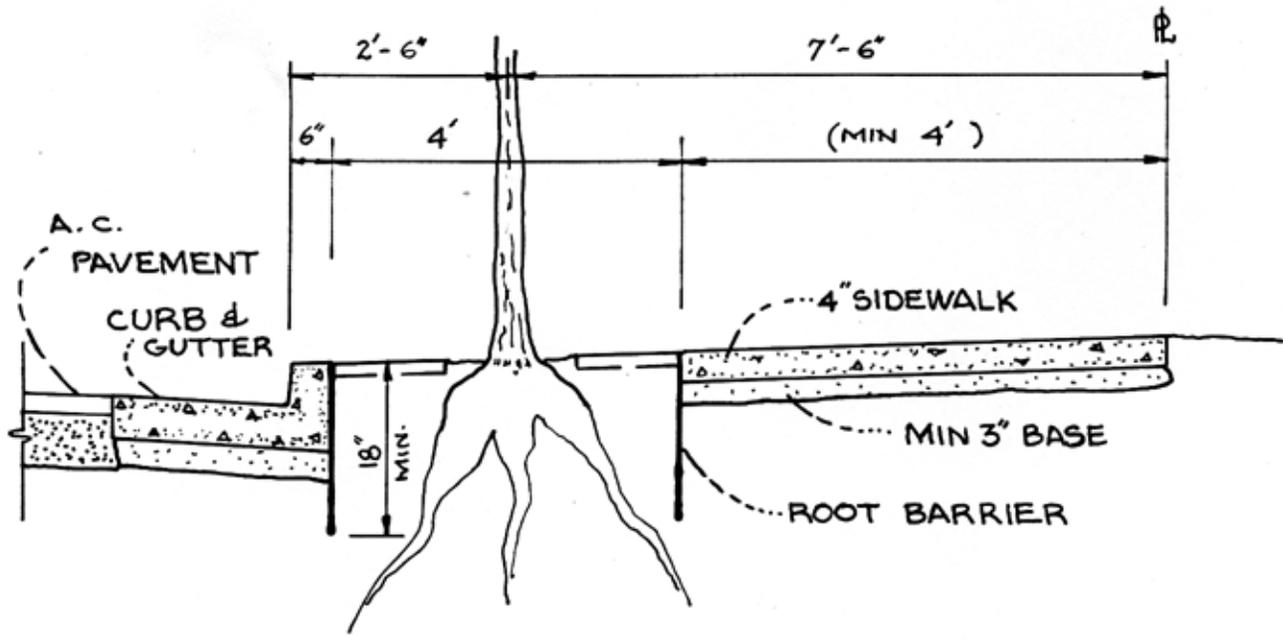
Approved By
City Administrator
Robert...
Date
MAY 29, 1996

Approved By
City Engineer
Andrew Boyd
Date
FEB 22, 1996

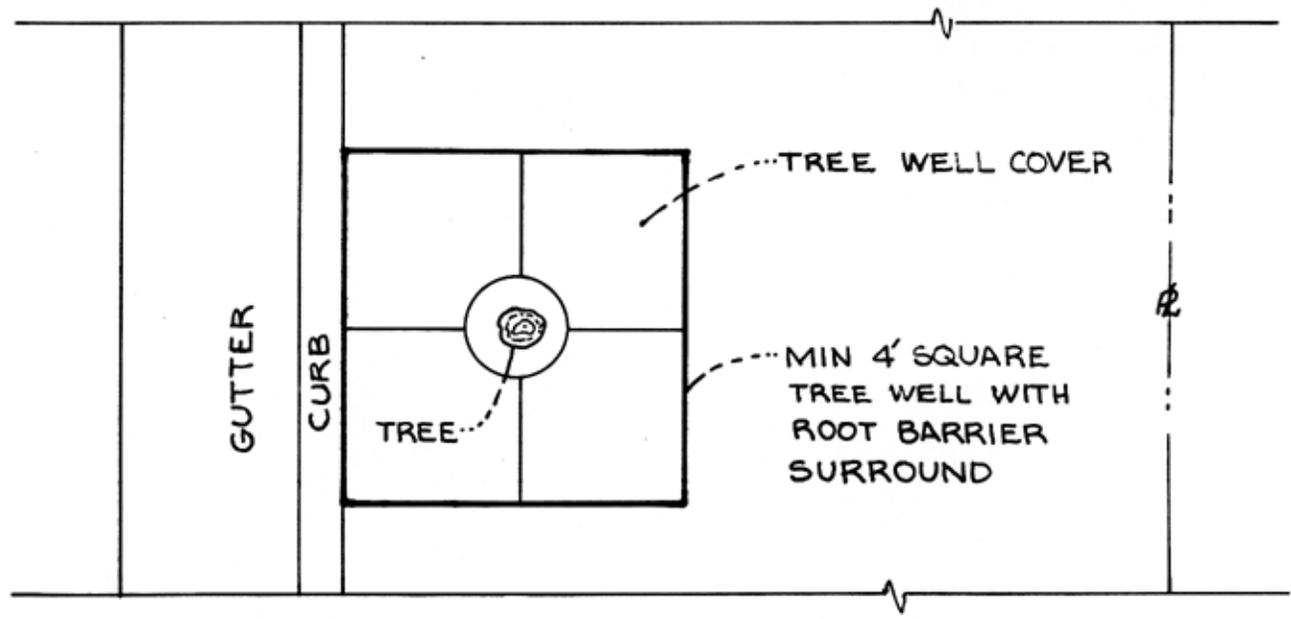
RESIDENTIAL TREE - TYPE 'B'

No.	Date	App.	Revision

Standard Detail No.
L-4
4 OF 5



SECTION



PLAN VIEW



Approved By
City Administrator
[Signature]
Date
MAY 29, 1996

Approved By
City Engineer
[Signature]
Date
FEB 22, 1996

COMMERCIAL TREE

No.	Date	App.	Revision

Standard Detail No.
L-5
5 OF 5



CITY OF BISHOP

377 West Line Street - Bishop, California 93514

Post Office Box 1236 - Bishop, California 93515

760-873-8458 publicworks@ca-bishop.us

www.ca-bishop.us

Tree Care Information

Planting Trees

- Most trees need to be planted in a hole exactly as deeply as they sat in their containers (pots). If planted too deep, for example, they will slowly die.
- Dig a hole as deep and twice as wide as the container (larger if there are large rocks or if the soil is unusually hard).
- If the soil is very dry or if you suspect poor drainage, fill with water and let the water go down. If it takes hours to drain, consult with a nursery or landscaping expert about helping with soil drainage.
- When the trees are first delivered, the roots are contained in a space the size of the containers they arrived in. This concentration of roots is called a root ball.
- Set in the tree and use a straight stick or shovel handle to make sure the top of the root ball is level with the soil. Fill in the bottom of the hole to adjust the grade, if needed, and tamp firmly.
- Fill in the hole around the root ball with a 50-50 mix of soil and soil amendment. Starter fertilizer should also be placed near the roots. Starter fertilizers are low nitrogen, high phosphorus products that help root growth without promoting new top growth. You need to help get the roots out before there are new leaves to feed. There are granular forms, organic fertilizers (even bone meal is good), and tablets made for this purpose.
- Fill the hole but do not cover the root ball with more dirt. Wet and mix the soil thoroughly. Sloshy mud makes good contact with the roots, which helps root growth out into the surrounding soil.

Staking Trees

- Staking trees not only prevents trees from blowing over or snapping in the wind, but also keeps the root ball from wobbling in the planting hole which can break off new roots.
- Use two stakes per tree, typically one on the north and one on the south side.
- Drive stakes in past the depth of the root ball and just outside the width of the root ball. Do not pierce the root ball with the stakes.

- Stakes can be driven before backfilling the planting hole.
- Tie stakes to the tree at two locations, from both sides, using four ties altogether.
- Use 1 to 1.5 inch square stakes and 1 inch wide green plant tape as ties for 5 gallon trees.
- Use 2 to 2.25 inch diameter lodge poles for stakes and use rubber ties that are nailed to the stakes for 15 gallon trees.
- Any nursery can show you which lodge pole length you need for your tree and how to attach ties.
- Make a well or depression around the tree to aid in watering.

Tree Care

- Water new trees frequently – more than they will need when they are established.
- Water the root ball of the tree. Many assume that if the soil several feet away looks moist that the root ball is moist also and that if the surface is wet that the soil 18 inches down is wet too. This is often not the case.
- Fill the well with water at least daily. If the water soaks in quickly, fill it again. Just sprinkling the ground surface with a hose nozzle is not likely to reach the thirsty roots. One can get an idea how well your watering is working by probing the root area with a finger to see if it is dry.
- Water the tree deeply. As the tree grows, deep watering will insure deep rooting with numerous benefits. The tree will be watered deeply if the water soaks slowly and deeply into the ground. Even if a lawn around the tree is watered, lay a slowly dripping hose by the tree twice a week to water the tree deeply and to promote deep roots.
- Continue to water the tree this way for the first year or two.
- After 30 to 60 days apply an all-purpose fertilizer, a "15-15-15" or "16-16-16", or a "4-4-4" or similar if organic. Liquid fertilizer needs to be applied more frequently than other forms.
- Continue to fertilize the tree this way in the spring and fall of each year.

This information is available of the City of Bishop website www.ca-bishop.us



CITY OF BISHOP

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Approved Street Tree Varieties

List A (no power lines)

Technical Name	Common Name
Ginkgo biloba	Maidenhair Tree
Koelreuteria paniculata	Golden Rain Tree
Liriodendron tulipifera	Tulip Tree
Pyrus calleryana 'Aristocrat'	Flowering Pear
Sophora japonica	Pagoda Tree
Ulmus parvifolia	Chinese Elm
Zelkova serrata	Sawleaf Zelkova

List B (anywhere)

Technical Name	Common Name
Cercis canadensis	Eastern Redbud
Cercis reniformis "Oklahoma"	Oklahoma Redbud
Lagerstroemia indica	Crape Myrtle
Pistacia chinensis	Chinese Pistache
Robinia ambigua "Idahoensis"	Idaho Locust



Project Study Report (PSR)
Warren Street Improvements Project

Approved:

David Grah
Director of Public Works

1 January 2011

Date

This PSR is prepared and intended to meet state requirements.

1. Problems

This project is to address deteriorated pavement, poor drainage, no or deteriorated curb and gutter, lack of sidewalk, curb returns not accessible to disabled, lack of coherent and aesthetic theme.

2. Location

This project is located in Bishop on North and South Warren Street from West South Street to dead end near West Elm Street, a distance of about 2,700 feet, and on Lagoon Street, West Pine Street, and West Elm Street between Warren Street and Main Street, a combined distance of about 750 feet. See project map. West South Street, Church Street, and Academy Street between Warren and Main could be included in the project as options, for an additional combined distance of 750 feet.

3. Scope

This project will:

- Remove and replace pavement 40 feet wide where needed
- Improve roadway grade where needed
- Remove existing curb, deteriorated curb and gutter or curb and gutter on poor grade and replace with curb and gutter
- Reconstruct and extend storm drain between Lagoon Street and Academy Street and provide oil / water separator
- Construct concrete cross gutters
- Construct continuous Americans with Disabilities Act (ADA) compliant 10 foot sidewalk as right of way allows

- Construct ADA compliant curb ramps
- Remove trees that conflict with proposed sidewalk
- Provide street trees and irrigation
- Provide architectural street lighting
- Improve intersections with Church Street, West Pine Street, and Grove Street as necessary to address drainage problems and grade issues
- Consider bulb-outs at intersections to provide enhanced pedestrian refuge, traffic calming, and context sensitive elements
- Consider aesthetic enhancement of overhead utilities
- Water and sewer improvements are anticipated in conjunction with this street project

4. Street Classification

The City of Bishop General Plan classifies the streets involved in the project as a Commercial Streets. West South Street and West Elm Street are federally classified as Urban Collectors. These city streets are not on the National Highway System.

Federal-aid Classification

<input type="checkbox"/>	Urban Principal Arterial	<input type="checkbox"/>	Rural Principal Arterial
<input type="checkbox"/>	Urban Minor Arterial	<input type="checkbox"/>	Rural Minor Arterial
<input checked="" type="checkbox"/>	Urban Collector	<input type="checkbox"/>	Rural Major Collector
<input checked="" type="checkbox"/>	Urban Local (ineligible)	<input type="checkbox"/>	Rural Minor Collector (ineligible)
<input type="checkbox"/>	Rural Local (ineligible)		

5. Environmental

CEQA: A California Environmental Quality Act (CEQA) Negative Declaration (ND) is anticipated.

NEPA: If Federal funds are obtained, a National Environmental Policy Act (NEPA) Finding of No Significant Impact is anticipated.

Potential Issues:

- Parking impacts for adjoining properties
- Business and driveway access
- Removal of mature shade trees
- Construction noise and dust
- Inconvenience during construction

6. Traffic Data

Current Estimate Average Daily Traffic:	2,000
% Trucks:	10
Current Design Hourly Volume:	500

7. Roadway Geometry

This project will not significantly change the width or alignment of the existing streets. Small adjustments to width and alignment may be necessary to match existing right of way. A slight grade change is anticipated to improve drainage. Curb returns will use a 10 foot radius unless bulb-outs are used at intersections.

8. Bridges

There are no bridges on this project.

9. Condition of Existing Facility

The existing pavement is deteriorated, has extensive cracking, and experiences flooding during rain, snow melt, and excessive irrigation. The grades along Warren Street are very flat and this, coupled with no gutters, missing, poor, and displaced curbs, and limited storm drain contributes to street flooding.

Existing curb is not continuous nor does it include a gutter. This hinders drainage and promotes the growth of weeds along the curb face. There are few concrete cross gutters at intersections. The existing trees are not species approved as street trees in the City of Bishop and roots damage sidewalks and other improvements. There is little sidewalk and some of the sidewalk that exists is not ADA-compliant. Curb ramps are not ADA-compliant.

10. Pavement Rehabilitation

The pavement work will provide a service life of at least 10 years. A standard pavement section used throughout the city is anticipated. This pavement section will be verified using materials tests and Caltrans processes. This project is consistent with the City of Bishop Pavement Management Plan.

11. Consequences of Not Doing Project

If this project is not constructed pavement will continue to deteriorate, flooding will continue, pedestrians and disabled will continue to not be served on this street, and the important commercial neighborhood will continue to lack a coherent and aesthetic theme. If this project is not constructed the public's investment in assets will not be protected and public liability related to deteriorated pavement, water ponding, freezing, displaced curbs, lack of pedestrian and disabled accessibility will increase, and commercial vitality will be limited.

12. Costs and Schedule

Excluding water and sewer improvements, project costs and schedule are estimated as follows for all locations considered for the project and assume project funding becomes available in August 2011:

Element	Cost	Start	Finish
Environmental Analysis	\$144,000	August 2011	March 2012
Project Design	\$144,000	August 2012	May 2013
Construction	\$3,300,000	September 2013	December 2013
Total	\$3,588,000		

Costs are January 2011. Costs and schedule are based on state funding. If federal processes are involved, additional costs and additional time will be required. Support costs should be escalated at 3% per year and capital costs should be escalated at 5% per year.

13. Other Agencies

No significant involvement by other agencies is anticipated.

14. Consistency with Planning

This project is consistent with the Bishop General Plan.

15. Proposed Funding

Funding for this project is proposed to come primarily from dropping two other programmed City of Bishop street projects: the West Pine Street Improvements project and the Sneden Street Improvements project. There is \$2,430,000 available from these two projects for the proposed Warren Street Improvements project. Additional funding will be pursued to fully fund all locations considered for the project.

16. Attachments

- Project Map
- Typical Commercial Street Section

17. Report Preparation

This report was prepared by the City of Bishop Department of Public Works.